



# Alaska Department of Transportation and Public Facilities

Title VI and ADA  
A basic guideline to compliance  
Civil Rights Office

ALASKA DOT&PF CIVIL RIGHTS OFFICE



# Agenda

## Title VI Compliance Requirements

- What is Title VI
- Historical Content
- Disparate Treatment v. Impact
- Program Requirements and Assurances
- Complaint Procedures

## American with Disabilities Act

- Title II
- Title III
- Section 504 of the Rehabilitation Act
- Accessibility Standards

## Public Engagement



# What is Title VI?

## Title VI of the Civil Rights Act of 1964



“No person in the United States shall, on the ground of race, color, or national origin, be excluded from participation in, be denied the benefits of, or be subjected to discrimination under any program or activity receiving Federal financial assistance”

**Title VI, 42 U.S.C. § 2000d**



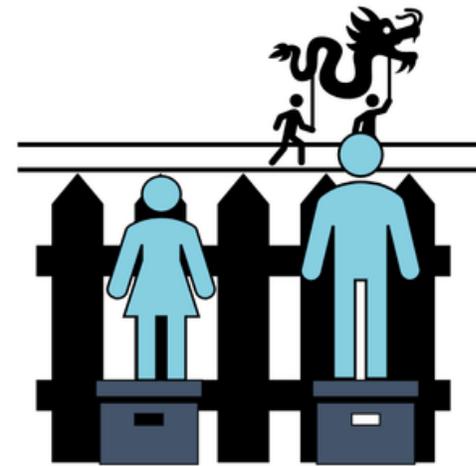
## Historical Context

- **Brown v. Board of Education (1954)**
  - A landmark unanimous ruling that racial segregation in public schools was unconstitutional.
- **Alexander v. Sandoval (2001)**
  - U.S. Supreme Court decision that significantly limited civil rights lawsuits by ruling that private individuals cannot sue to enforce regulations prohibiting "disparate impact" (unintentional discrimination) under Title VI of the Civil Rights Act of 1964

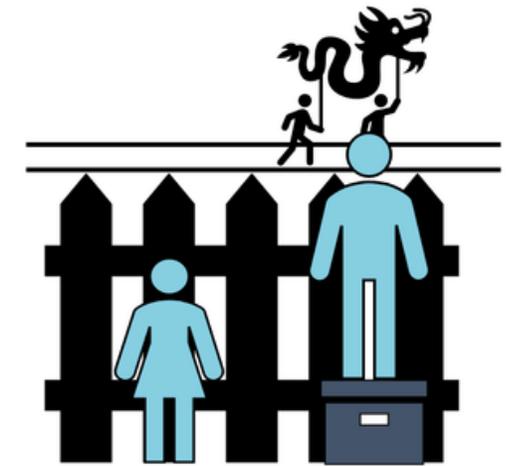


# Disparate Treatment v. Impact

- Disparate **treatment** involves (intentional) discrimination against individuals based on protected characteristics, while disparate **impact** refers to seemingly neutral policies that unintentionally, yet disproportionately, harm protected groups.
  - E.O. 14281
  - E.O. 12898 (\*rescinded)



Disparate Impact



Disparate Treatment



# Program Requirements and Assurances

- **Title VI Non-Discrimination Plan**

- 49 CFR Part 21 – State Assurances, Compliance, State Highway Agency Responsibilities, Data Collection and Analysis, Complaint Processing and Investigation and Limited English Proficiency Plan

- **Language Access Plan (LEP).**

- Four-Factor Analysis

Recipients should apply these four factors to determine the extent of their obligation to provide LEP services:

1. Number/proportion of LEP persons in a recipient's area.
2. Frequency of contact with LEP individuals for recipient's services.
3. Importance of the service provided by the recipient.
4. Resources available to the recipient and costs.

[Title VI of the Civil Rights Act of 1964, Civil Rights Office, Transportation & Public Facilities, State of Alaska](#)



# Place Based Analysis

## City of Hoonah

Current Population by Race:

Race	Percent of Population
American Indian or AK Native	42.24%
Asian	5.76%
Black or African American	2.0%
Native Hawaiian or Pacific Islander	0.0%
White	32.71%
Other Race	0.0%
Two or More Races	17.29%

Average Population (2015 - 2019): 784

## City of Gustavus

Current Population by Race:

Race	Percent of Population
American Indian or AK Native	6.09%
Asian	0.0%
Black or African American	0.0%
Native Hawaiian or Pacific Islander	0.0%
White	90.26%
Other Race	0.0%
Two or More Races	3.65%

Average Population (2015-2019): 545

## City of Tenakee Springs

Current Population by Race:

Race	Percent of Population
American Indian or AK Native	0.0%
Asian	0.0%
Black or African American	0.0%
Native Hawaiian or Pacific Islander	0.0%
White	100.0%
Other Race	0.0%
Two or More Races	0.0%

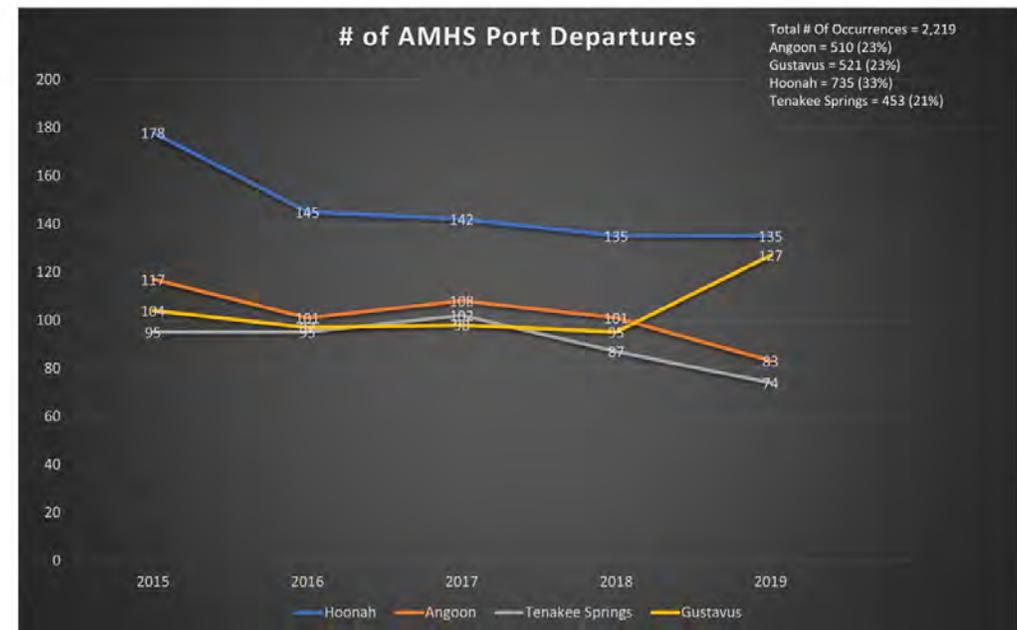
Average Population (2015-2019): 140

## City of Angoon

Current Population by Race:

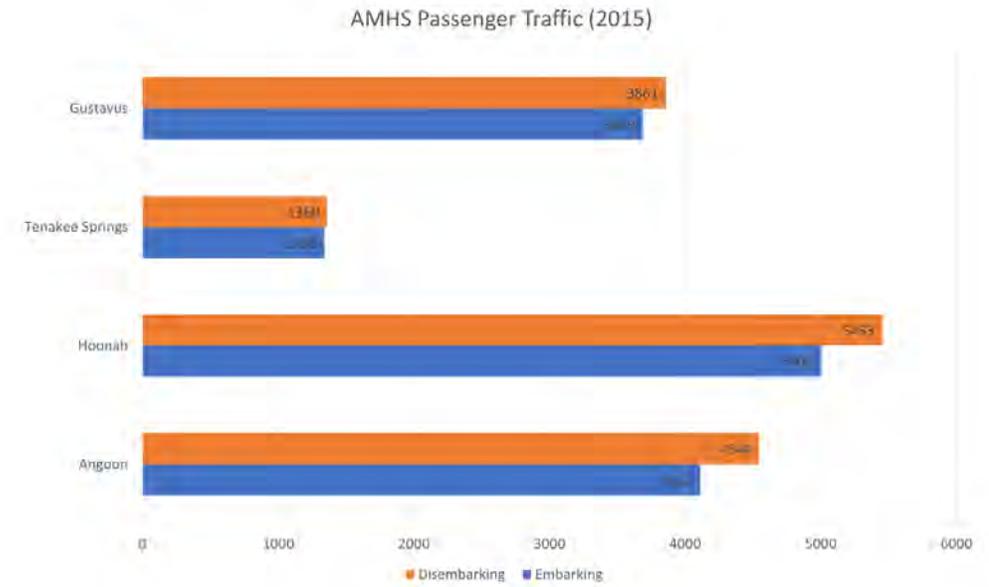
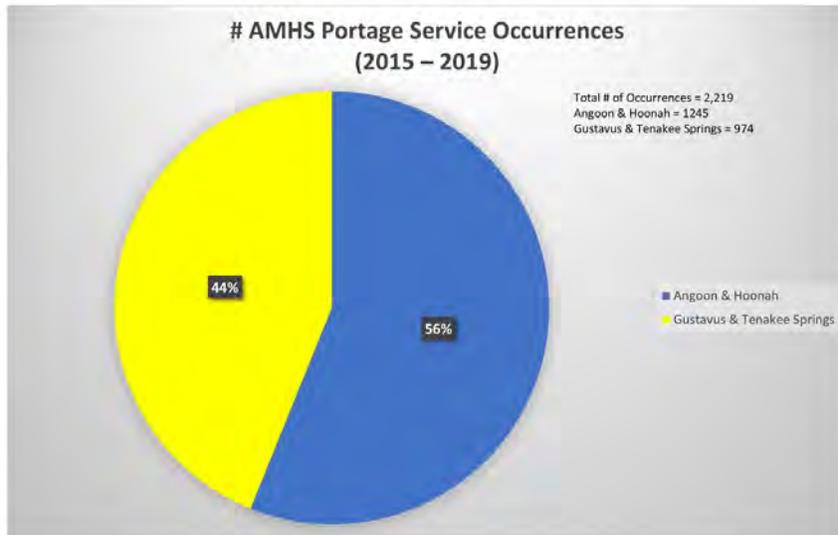
Race	Percent of Population
American Indian or AK Native	54.93%
Asian	0.0%
Black or African American	11.07%
Native Hawaiian or Pacific Islander	0.0%
White	19.92%
Other Race	4.23%
Two or More Races	9.86%

Average Population (2015-2019): 410





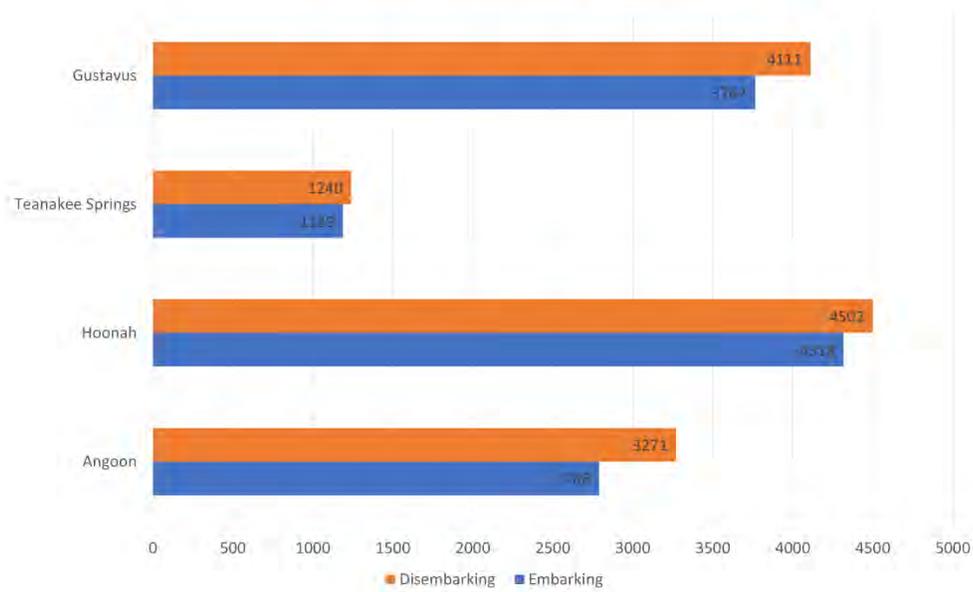
# Placed Based Analysis



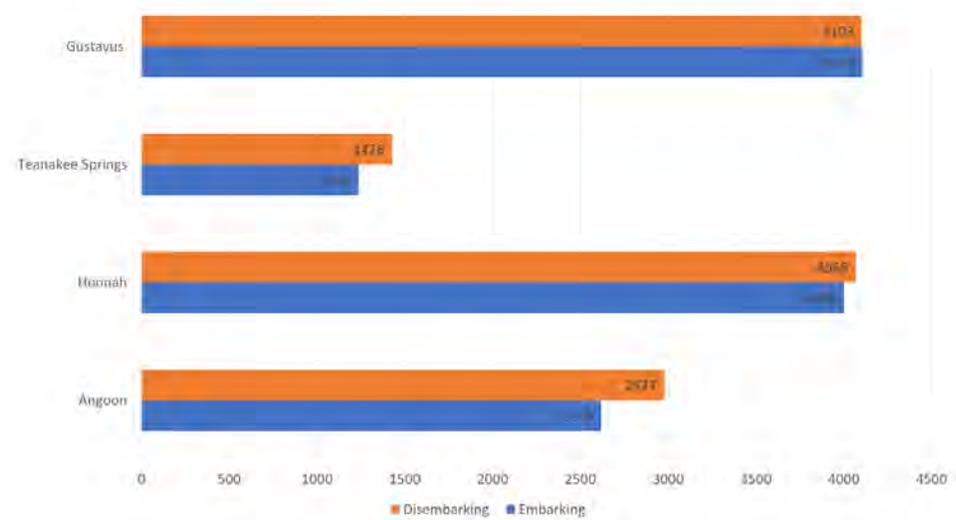


# Placed Based Analysis

AMHS Passenger Traffic (2016)



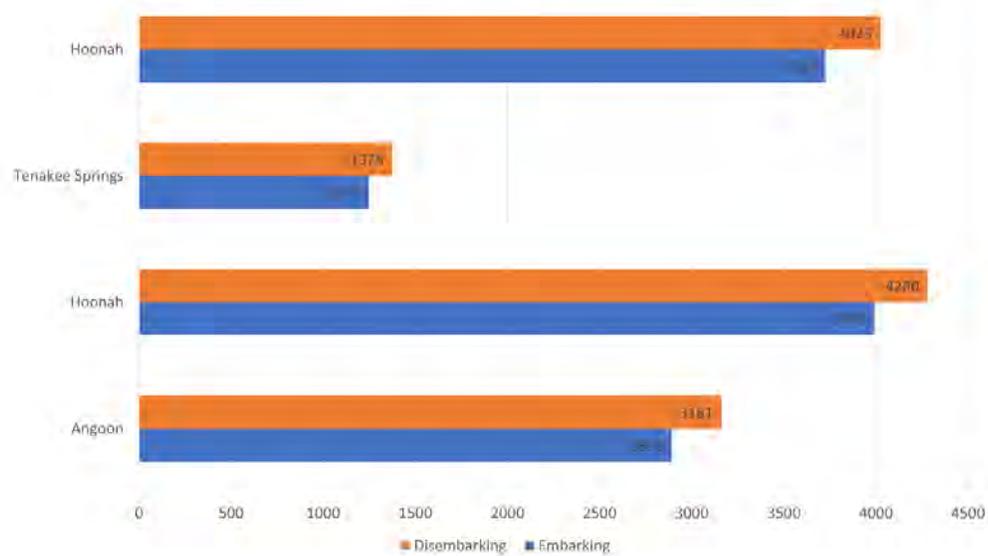
AMHS Passenger Traffic (2017)



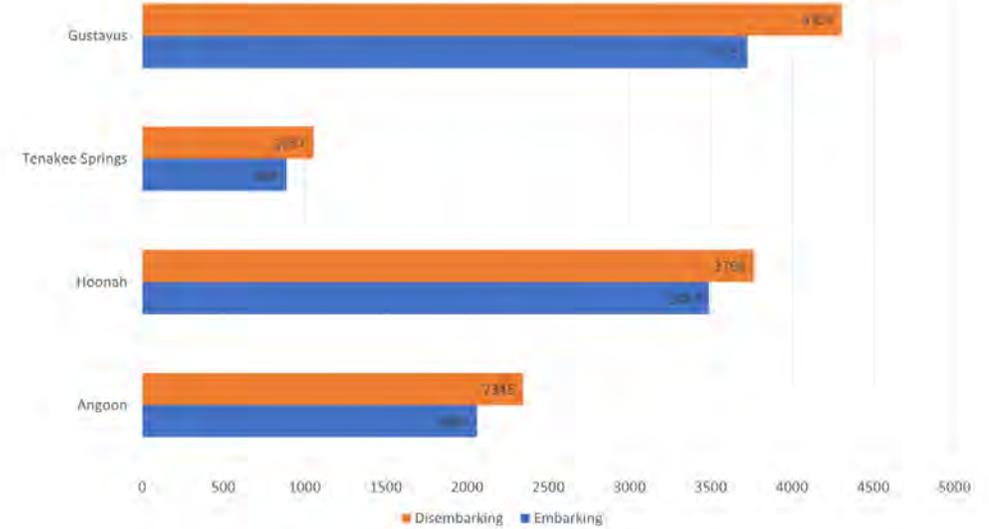


# Placed Based Analysis

AMHS Passenger Traffic (2018)

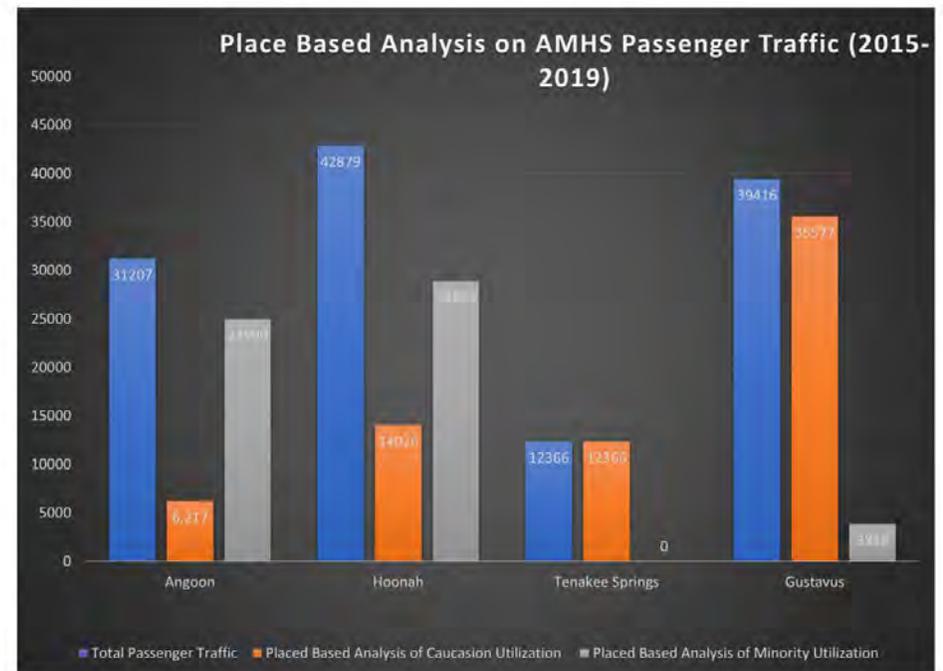
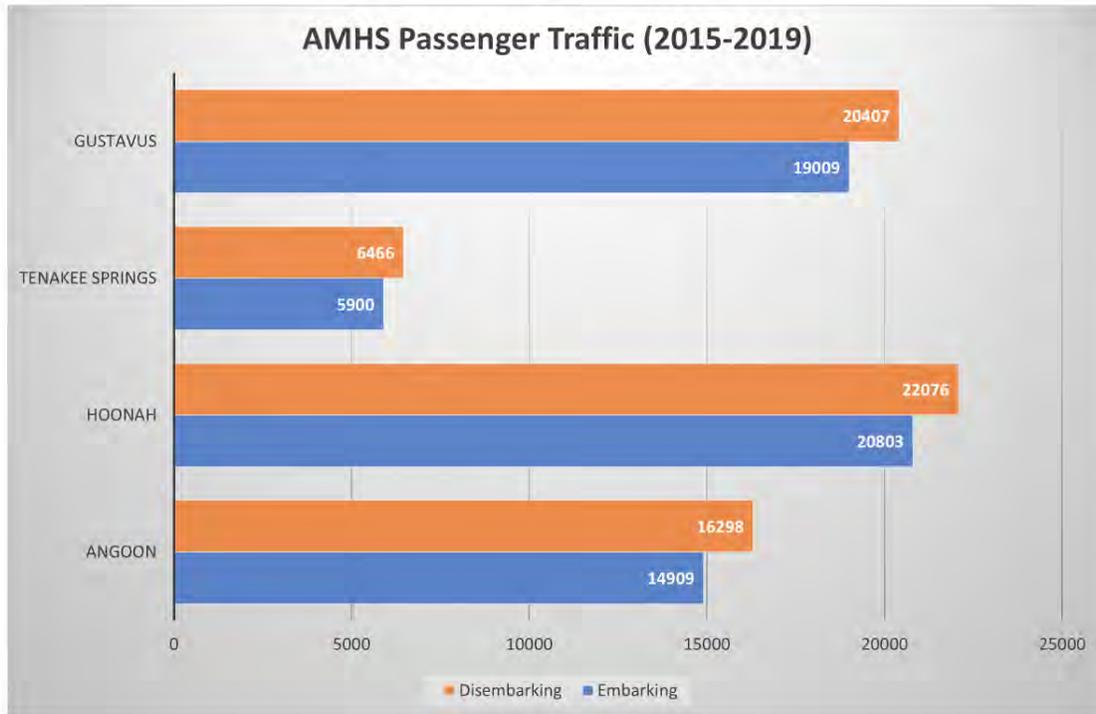


AMHS Passenger Traffic (2019)



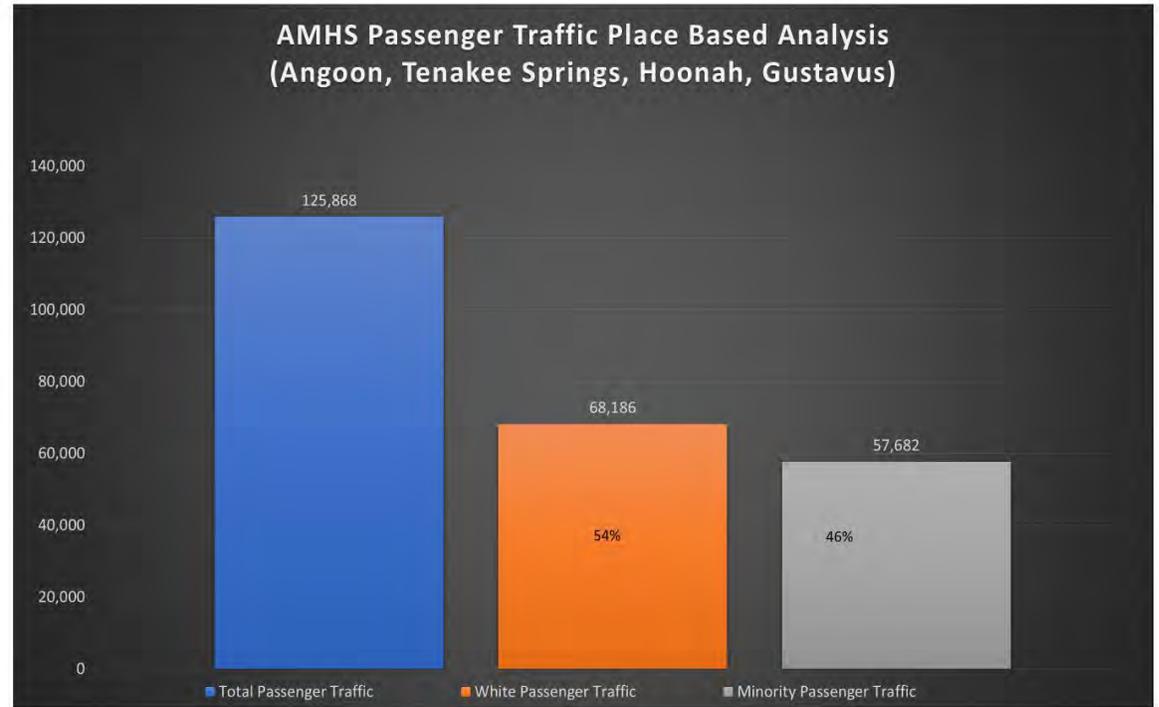
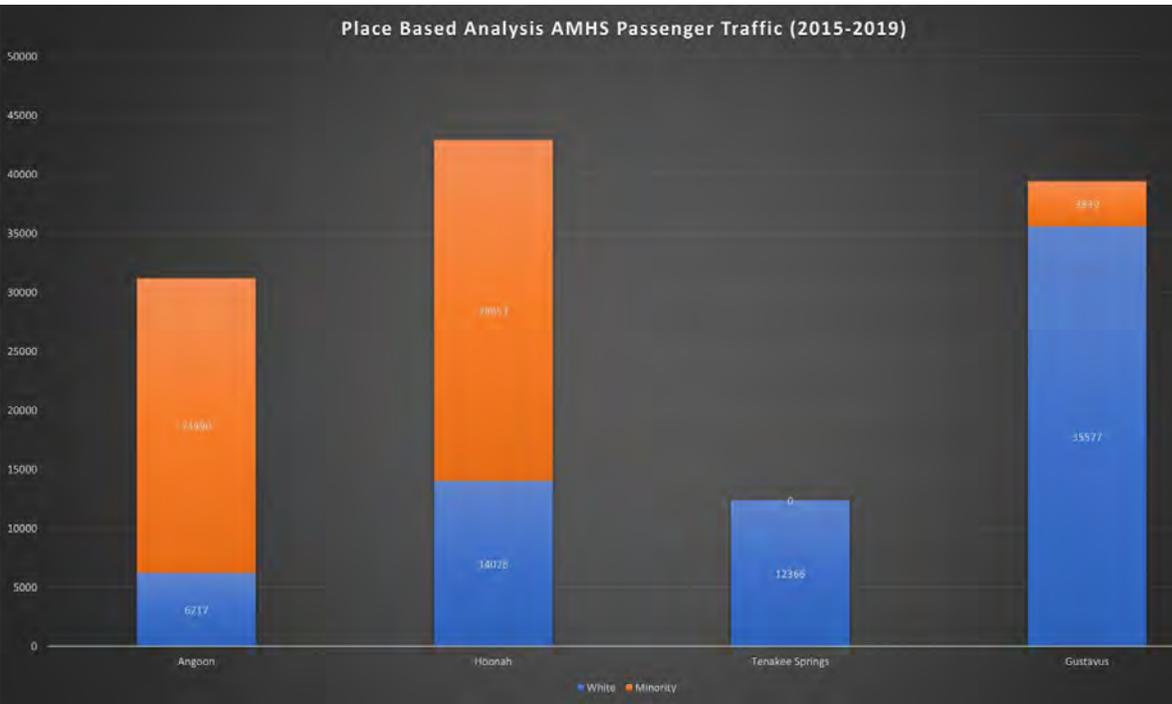


# Placed Based Analysis



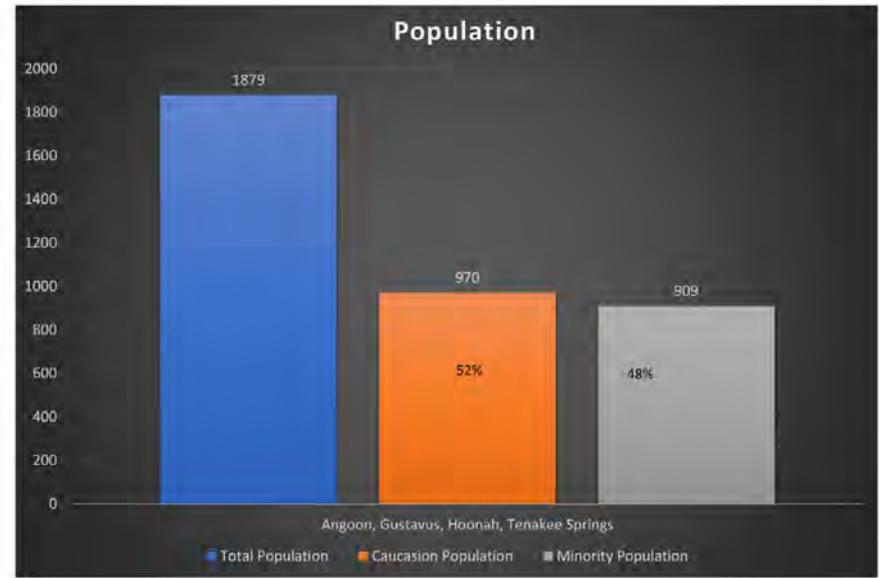
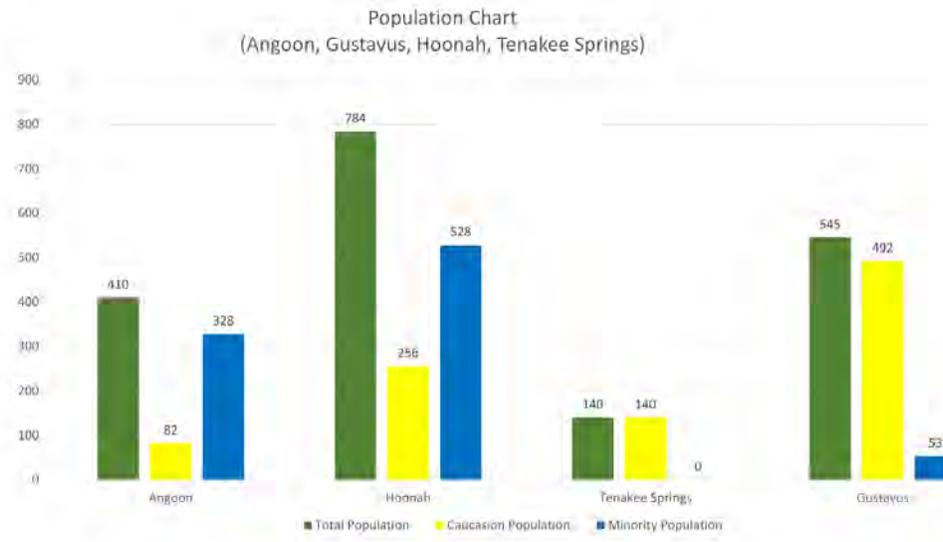


# Placed Based Analysis





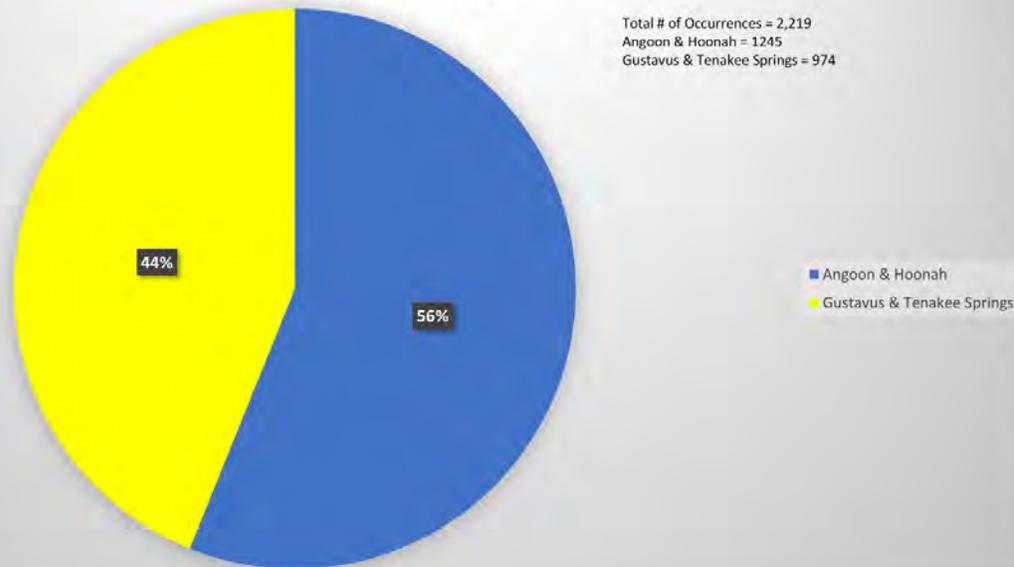
# Placed Based Analysis





# Placed Based Analysis

# AMHS Portage Service Occurrences  
(2015 – 2019)



Total # of Occurrences = 2,219  
 Angoon & Hoonah = 1245  
 Gustavus & Tenakee Springs = 974

**Equity Ratio** – The **percentage of costs** (impact or burden) absorbed by a demographic group **divided by** their **percentage of the total population**.

**Data:**

- Total Population = 1,879
- White Population = 970
- Minority Population = 909
- Total **Impact Units** = 125,863
- White **Impact Units** = 68,186
- Minority **Impact Units** = 57,862

**Equity Ratio Formula:**

$$\text{Equity Ratio} = \frac{\text{Group's \% of Costs}}{\text{Group's \% of Population}}$$

$$\text{White Equity Ratio} = \frac{(68,186 / 125,863)}{(970 / 1,879)} = \frac{0.541}{0.516} = \mathbf{1.049}$$

$$\text{Minority Equity Ratio} = \frac{(57,862 / 125,863)}{(909 / 1,879)} = \frac{0.460}{0.484} = \mathbf{0.94}$$

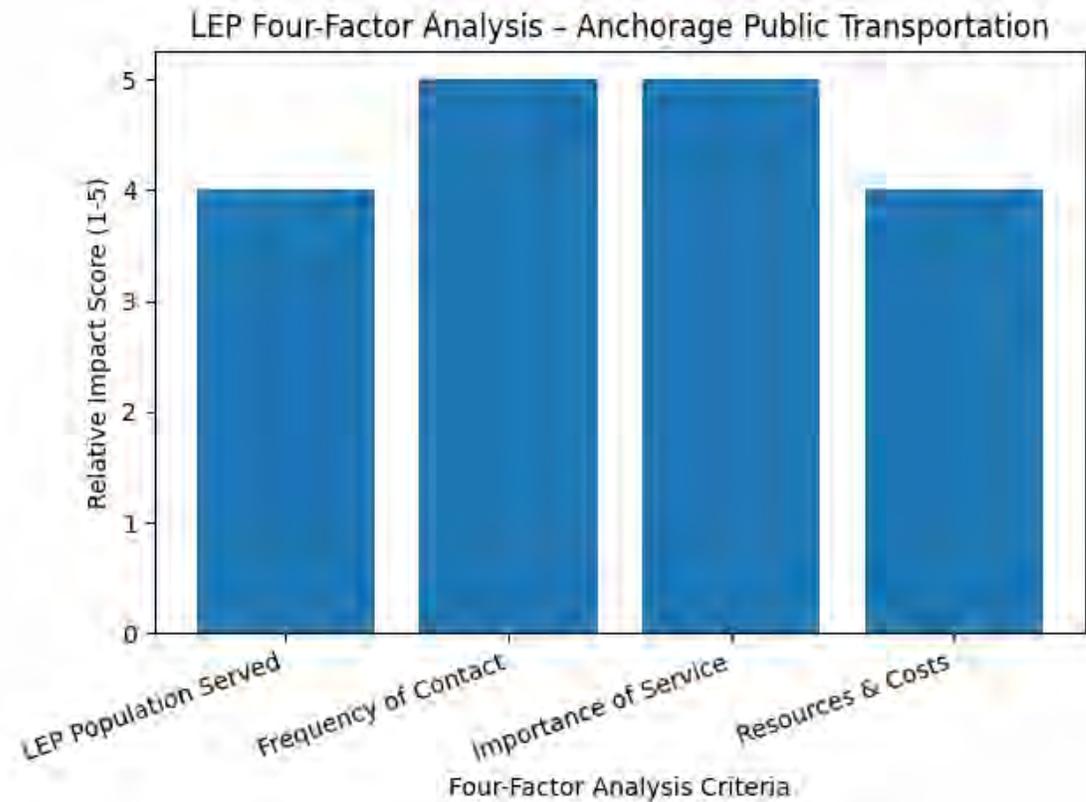
**How to Interpret:**

- **White Equity Ratio = 1.049** → Slightly more burdened than their population share.
- **Minority Equity Ratio = 0.94** → Slightly less burdened than their population share.



# Four Factor Analysis

- **Factor 1: Number or Proportion of LEP Persons Served or Encountered**
  - Spanish
  - Tagalog
  - Samoan
  - Korean
  - Hmong
  - Alaska Native languages (e.g., Yup'ik, Inupiaq)
- **Factor 2: Frequency of Contact with LEP Individuals**
  - Daily bus boarding and fare transactions
  - Rider questions regarding routes and schedules
  - Service alerts, detours, and emergency notifications
  - Customer service communications (phone, in-person, and online)
- **Factor 3: Nature and Importance of the Program, Activity, or Service**
  - Employment opportunities
  - Medical appointments and health services
  - Educational institutions
  - Government and social service programs
  - Essential goods and community resources
- **Factor 4: Resources Available and Costs**
  - Multilingual printed materials (routes, schedules, fare information)
  - Interpretation services via phone or contracted vendors
  - Website translation and digital rider alerts
  - Partnerships with community-based and cultural organizations
  - Staff training on language access procedures





# Complaint Procedures

THE GREAT STATE of ALASKA myAlaska Departments State Employees

Alaska Department of Transportation and Public Facilities  
**CIVIL RIGHTS OFFICE** SEARCH DC

Travel Business News and Social Projects About Us

You are here: DOT&PF > Civil Rights Office > Title VI

Home Programs ETA Subrecipient FAA Events Newsletter Forms Bidding Archives DBE Directory Contact

**Title VI of the Civil Rights Act of 1964** [File a Complaint](#)

**Alaska DOT&PF Title VI and ADA Coordinator**

**Robespierre Howard**  
Title VI and ADA Coordinator  
Alaska Department of Transportation, Civil Rights Office  
2200 E. 42nd Ave.  
Anchorage, AK 99508  
Phone (voice): (907) 269-0852  
Phone (TTY): 711 for Alaska Relay  
Fax: 907-269-0847  
Email: [robspierre.howard@alaska.gov](mailto:robspierre.howard@alaska.gov)

**Goals & Accomplishments 2024**

[Title VI Program Annual Goals & Accomplishments Report 2024](#) **NEW!**

**ADA & Title VI Policies (English & Translated)**

- [Spanish](#)
- [Tagalog](#)
- [Korean](#)
- [Russian](#)
- [Vietnamese](#)
- [English](#)

**Title VI Documents**

- [Title VI Language Identification](#)
- [Title VI Brochure](#)
- [Title VI Public Meeting Report](#)
- [Title VI Public Sign-In Sheet](#)
- [Title VI Demographics and Statistics](#)
- [Title VI Basic Training](#)
- [Spanish Brochure](#)
- [Tagalog Brochure](#)
- [Hmong Brochure](#)
- [Korean Brochure](#)
- [Yup'ik Brochure](#)
- [Inupiat Brochure](#)
- [Russian Brochure](#)
- [English Brochure](#)

**Resources**

- [Title VI Essentials](#)
- [FHWA's "How to Engage Low-Literacy and Limited-English-Proficiency Populations in Transportation Decisionmaking"](#)
- [AMATS \(Anchorage Metropolitan Area Transportation Solutions\)](#)
- [FMATS \(Fairbanks Metropolitan Area](#)

- Title VI procedures and policies, including the complaint procedure: [Title VI of the Civil Rights Act of 1964 & Environmental Justice, Civil Rights Office, Transportation & Public Facilities, State of Alaska](#)



# What is the ADA?



- Americans with Disabilities Act of 1990

Requires state and local governments to ensure programs and activities (like public transit) are accessible to individuals with disabilities.

- Five Titles – focus: Title II (State & Local Government) and Title III (Reasonable Accommodations)

[49 CFR Part 38, Accessibility Specifications for Transportation Vehicles](#)

[Appendix A to 49 CFR Part 37, ADA Standards for Transportation Facilities.](#)

- [Chapter 8 of FTA ADA Circular 4710.1](#)

Provides additional guidance and discussion on the U.S. DOT ADA requirements for transportation facilities of FTA grantees.



# Title II, III, and Sec. 504 of the Rehabilitation Act

- Title II of the ADA
  - Requires state and local governments to ensure programs and activities are accessible to individuals with disabilities.
    - Subtitle A
    - Subtitle B
- Section 504 of the Rehabilitation Act
  - Mandates that recipients of federal funding must comply with the same accessibility standards as public entities.
- Title III of the ADA
  - Focuses on ensuring that private businesses and public accommodations are accessible to individuals with disabilities.



## Title II of ADA

- [Title 49 Part 38 Subtitle A](#)

- Title II is divided into two subtitles: Subtitle A protects qualified individuals from discrimination in services, programs, or activities, including employment, extending requirements similar to Section 504 of the Rehabilitation Act to all public entities.

- [49 CFR Part 38 Subpart B -- Buses, Vans and Systems](#)

- Subtitle B covers public transportation systems, including commuter and intercity rail, establishing operational standards. **The Department of Transportation oversees Subtitle B implementation.**



## Section 504 of the Rehabilitation Act

- **Section 504 of the Rehabilitation Act of 1973** states that no qualified individual with a disability shall, solely by reason of their disability, be excluded from participation in, denied the benefits of, or subjected to discrimination under any program or activity receiving federal financial assistance.
  - A national law that protects qualified individuals from discrimination based on their disability
  - Widely recognized as the first civil rights statute for people with disabilities setting precedent for the American with Disabilities Act of 1990



## Title III of ADA

- Title III of the ADA specifically addresses discrimination in public accommodations and commercial facilities.
  - Accessibility Standards
  - Removal of Barriers
  - Auxiliary Aids and Services
  - Digital Accessibility
    - Ensure accessible websites, documents, and public notices
    - Use captioning and alternative formats
    - **April 6, 2026, WCAG**



# Accessibility Standards

	<b>PROWAG (Pedestrian Accessibility Route)</b>	<b>2010 ADAAG (Accessible Route)</b>
Width	Minimum 48 inches required in right-of-way; no exceptions for temporary width reduction. (R302.2)	Minimum 36 inches with allowance to reduce to 32 inches for short lengths. (403.5.1.)
Grade	Can follow the grade of adjacent roadway without the requirement of ramps (handrail or edge protection). (R304.2.1.) (R407.1)	Ramps required where grade is between 5% and 8.33%. Requires handrails and edge protection where the rise is greater than 6 inches. Rise is limited to 30 inches before a landing is required. (403.3, 405.6, 405.8)
Curb ramp match points	Guidelines provide a maximum 15-foot cutoff for ramp length and allowance of grades greater than 8.33%. (R304.2.1.)	No cutoff distance noted. Grade cannot exceed 8.33%. (405.2)
Accessibility on-street parking	Parallel on-street parking spaces shall connect to pedestrian access routes. Parking spaces must be a minimum of 24 feet long and 13 feet wide and not encroach on the traveled way. (R310)	Nothing stated in the ADA Guidelines.
Detectable warning fields	Required at street and railroad crossings and unprotected transit stops. (R305.2)	ADA refers to U.S. DOT requirements for detectable warning fields.
Pedestrian crossing cross slope	Cross slope can be up to 5%. If the crossing is at mid-block, cross slope can match the road grade.	The maximum cross slope is 2.1%. (405.3)

The U.S. Access Board

ADAGG

PROWAG

Additional information about the Access Board can be found on their [website](#). Information about the Access Board's guidelines for [transportation vehicles](#) and [facilities](#) can be found on their website.



- **Curb Ramps:** Required whenever a project constructs new pedestrian facilities or alters a street through resurfacing. Standard running slopes should not exceed 1:12 (8.33%).
- **Bus Boarding Areas:** Must have a firm, stable surface with minimum dimensions of 96" long by 60" wide and a slope not exceeding 1:48.
- **Accessible Routes:** New facility construction must ensure at least 60% of public entrances are accessible.
- **Work Zone Compliance:** Construction zones must provide safe, continuous guidance for pedestrians, ensuring barriers do not create inaccessible "dead ends".



## Key Construction Requirements

- **ADA Transition Plan:** A "living document" that identifies physical barriers and outlines a schedule and budget for their removal.
- **Self-Assessment:** Public entities must periodically review their infrastructure to identify non-compliant areas for future prioritization.



myAlaska Departments State Employees

Western Alaska Storm 2025 - Updates Here

ALASKA DEPARTMENT OF TRANSPORTATION and PUBLIC FACILITIES

511 Alaska

Check driving conditions of Alaska's roadways on the 511 app or at 511.Alaska.gov

KNOW B4 U GO

What are you looking for?

<p><b>TRAVEL</b></p> <p>AIR ROAD SEA</p> <p>AMHS Ferries Travel Maps 511 Info Road Weather DMV Report Pothole Winter Season More...</p>	<p><b>PROJECTS</b></p> <p>REGIONS</p> <p>Project List Project Status EGIS Contracting Public Notices Northern Central Southcoast More...</p>	<p><b>SAFETY</b></p> <p>PROGRAMS</p> <p>Report a crash Safety Programs Safe Routes Highway Safety Snowplow Safety More...</p>
<p><b>BUSINESS</b></p> <p>CONTRACTS</p> <p>Commercial Vehicles Contracting Permits Leasing Facilities Civil Rights/DBE AASHTOWare More...</p>	<p><b>MEDIA</b></p> <p>PRESS</p> <p>Newsroom Contacts Twitter Instagram Facebook YouTube Vimeo Flickr More...</p>	<p><b>DIVISIONS</b></p> <p>JOBS</p> <p>Prog Mgmt&amp;Admin AMHS Aviation Civil Rights Comm't Facilities M&amp;O SWPlanning More...</p>

## Accessibility Standards

- Reasonable Accommodations
- Grievance Procedures (28 CFR §35.107)
- Coordinator Contact



## Public Engagement

- **ADA Transition Plan:** A "living document" that identifies physical barriers and outlines a schedule and budget for their removal.
- **Self-Assessment:** Public entities must periodically review their infrastructure to identify non-compliant areas for future prioritization.

# Contact Information

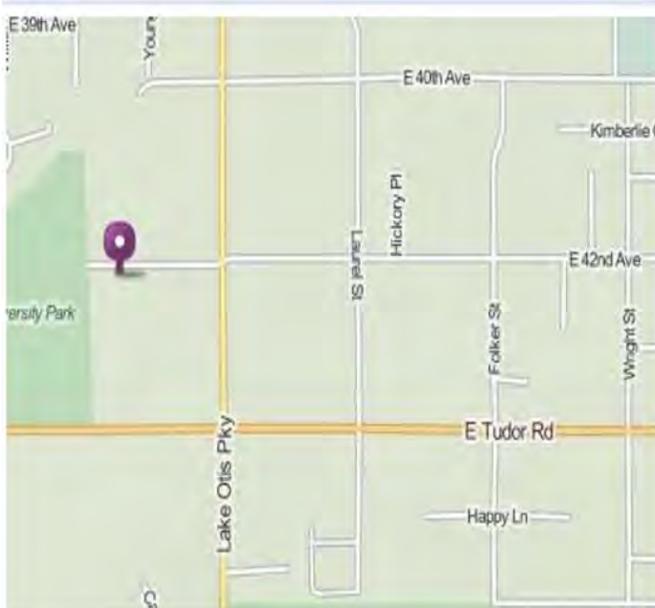
## Robespierre Howard

Alaska DOT&PF Civil Rights Office

Title VI Specialist & ADA Coordinator

(907) 269-0852

[Robespierre.howard@alaska.gov](mailto:Robespierre.howard@alaska.gov)



Alaska DOT&PF  
Civil Rights Office  
2200 E. 42<sup>nd</sup> Ave  
PO Box 196900  
Anchorage, AK 99519-6900

Phone: (907) 269-0851

Fax: (907) 269-0847

[www.dotcivilrights.alaska.gov](http://www.dotcivilrights.alaska.gov)





Any  
Questions



## Navigating Accessibility

### ADAAG (Americans with Disabilities Act Accessibility Guidelines)

- Focus
- General foundation for non-discrimination based on disability
- Broad application

### PROWAG (Public Right-of-Way Accessibility Guidelines)

- Designed for pedestrian facilities
- Acknowledged as “best practice” by the FHWA (Federal Highway Administration)
- Clear specific standards



# Transition Plan

- Historically
  - Paper Based Surveys
  - CRO, Engineers, Interns
- Challenges
  - Time-consuming
  - Prone to error
  - Difficult to collect
  - Difficult to manage
  - Difficult to inspect

PROJECT NAME: Parks Hwy: Seward Meridian to Lucas  
 PROJECT NUMBER: 18-038  
 PRIMARY STREET: Seward Meridian Parkway  
 SECONDARY STREET: On Ramp NWN1  
 LEVEL MAKE/MODEL: \_\_\_\_\_

MARK LOCATION OF RAMP AND DRAW ARROW TO INDICATE DIRECTION OF NORTH

Notes: No PED button

MEASURED	CR ADA STANDARD
A 0.3	2.0% MAX
B 1.8	2.0% MAX
C 0.2	2.0% MAX
D 8.4	8.3% MAX
E	8.3% MAX
F	5.0% MAX
G 4.3	10.0% MAX
H 5.1	10.0% MAX
I 45"	36" MIN
J 60"	36" MIN
K 47"	36" MIN
L 57"	15.0" MAX

SECTION A-A  
CURB RAMP CURB AND GUTTER

- IS THE CURB RAMP CONSTRUCTED OF STABLE, FIRM, AND SLIP RESISTANT MATERIALS?  YES  NO
- ARE ANY VERTICAL CHANGES IN LEVEL LESS THAN 1/4" FOR ALL SURFACES AND GRADE BREAKS OF THE CURB RAMP?  YES  NO
- IS THE TRANSITION BETWEEN THE CURB AND THE GUTTER PAN SMOOTH?  YES  NO
- ARE ALL GRADE BREAKS PERPENDICULAR TO THE DIRECTION OF THE RAMP RUN?  YES  NO
- ARE RAMP, LANDINGS, AND GUTTER LINES DRAINING PROPERLY?  YES  NO
- IF DRAINAGE GRATES ARE LOCATED WITHIN THE PEDESTRIAN ACCESS ROUTE, DOES THE GRATE PROHIBIT PASSAGE OF A SPHERE GREATER THAN 1/2" IN DIAMETER? ARE ELONGATED OPENINGS PLACED SO THE LONG DIMENSION IS PERPENDICULAR TO THE DOMINANT DIRECTION OF TRAVEL?  YES  NO  N/A
- DOES A 24" DETECTABLE WARNING TILE STRIP EXTEND THE FULL WIDTH OF THE CURB RAMP OPENING AND ARE THE TRUNCATED DOMES ORIENTED FOR THE PREDOMINATE DIRECTION OF TRAVEL?  YES  NO
- IF MARKED CROSSWALKS ARE USED, IS A 48" X 48" CLEAR SPACE PROVIDED BEYOND THE BOTTOM GRADE BREAK, WITHIN THE PEDESTRIAN CROSSING, AND WHOLLY OUTSIDE THE PARALLEL VEHICLE LANE?  YES  NO  N/A
- DOES THE CURB RAMP COMPLY WITH CR ADA STANDARDS, AS OUTLINED ABOVE? IF THE ANSWER IS NO, COORDINATE WITH THE ENGINEER OF RECORD TO PROVIDE ADDITIONAL DOCUMENTATION DESCRIBING WHY.  YES  NO

STATE OF ALASKA  
DEPARTMENT OF TRANSPORTATION AND PUBLIC FACILITIES  
CENTRAL REGION

PRINTED NAME: \_\_\_\_\_ DATE: \_\_\_\_\_

PERPENDICULAR CURB RAMP

VERSION 2.0

PROJECT NAME: Parks Hwy: Seward Meridian to Lucas  
 PROJECT NUMBER: 18-038  
 PRIMARY STREET: Seward Meridian Parkway  
 SECONDARY STREET: On Ramp NWS1  
 LEVEL MAKE/MODEL: \_\_\_\_\_

MARK LOCATION OF RAMP AND DRAW ARROW TO INDICATE DIRECTION OF NORTH

Note: PED button surrounded by fugitive material

MEASURED	CR ADA STANDARD
A 1.5	2.0% MAX
B 4.2	2.0% MAX
C 0.2	2.0% MAX
D 5.8	8.3% MAX
E	8.3% MAX
F	5.0% MAX
G 5.5	10.0% MAX
H 4.9	10.0% MAX
I >36"	36" MIN
J >36"	36" MIN
K 37"	36" MIN
L 72"	15.0" MAX

SECTION A-A  
CURB RAMP CURB AND GUTTER

- IS THE CURB RAMP CONSTRUCTED OF STABLE, FIRM, AND SLIP RESISTANT MATERIALS?  YES  NO
- ARE ANY VERTICAL CHANGES IN LEVEL LESS THAN 1/4" FOR ALL SURFACES AND GRADE BREAKS OF THE CURB RAMP?  YES  NO
- IS THE TRANSITION BETWEEN THE CURB AND THE GUTTER PAN SMOOTH?  YES  NO
- ARE ALL GRADE BREAKS PERPENDICULAR TO THE DIRECTION OF THE RAMP RUN?  YES  NO
- ARE RAMP, LANDINGS, AND GUTTER LINES DRAINING PROPERLY?  YES  NO
- IF DRAINAGE GRATES ARE LOCATED WITHIN THE PEDESTRIAN ACCESS ROUTE, DOES THE GRATE PROHIBIT PASSAGE OF A SPHERE GREATER THAN 1/2" IN DIAMETER? ARE ELONGATED OPENINGS PLACED SO THE LONG DIMENSION IS PERPENDICULAR TO THE DOMINANT DIRECTION OF TRAVEL?  YES  NO  N/A
- DOES A 24" DETECTABLE WARNING TILE STRIP EXTEND THE FULL WIDTH OF THE CURB RAMP OPENING AND ARE THE TRUNCATED DOMES ORIENTED FOR THE PREDOMINATE DIRECTION OF TRAVEL?  YES  NO
- IF MARKED CROSSWALKS ARE USED, IS A 48" X 48" CLEAR SPACE PROVIDED BEYOND THE BOTTOM GRADE BREAK, WITHIN THE PEDESTRIAN CROSSING, AND WHOLLY OUTSIDE THE PARALLEL VEHICLE LANE?  YES  NO  N/A
- DOES THE CURB RAMP COMPLY WITH CR ADA STANDARDS, AS OUTLINED ABOVE? IF THE ANSWER IS NO, COORDINATE WITH THE ENGINEER OF RECORD TO PROVIDE ADDITIONAL DOCUMENTATION DESCRIBING WHY.  YES  NO

STATE OF ALASKA  
DEPARTMENT OF TRANSPORTATION AND PUBLIC FACILITIES  
CENTRAL REGION

PRINTED NAME: \_\_\_\_\_ DATE: \_\_\_\_\_

PERPENDICULAR CURB RAMP

VERSION 2.0



# ArcGIS Solution

- GIS (Geographic Information Systems)

- Field Forms
- Instant Data Capture
- Easily Adaptable
- Easily Accessible
- Easily Updatable

- Benefits

- Easier project planning
- Identify opportunities
- Identify non-compliance pre and post construction
- Easier data query

2025\_Curb\_Ramp\_Perpendicular

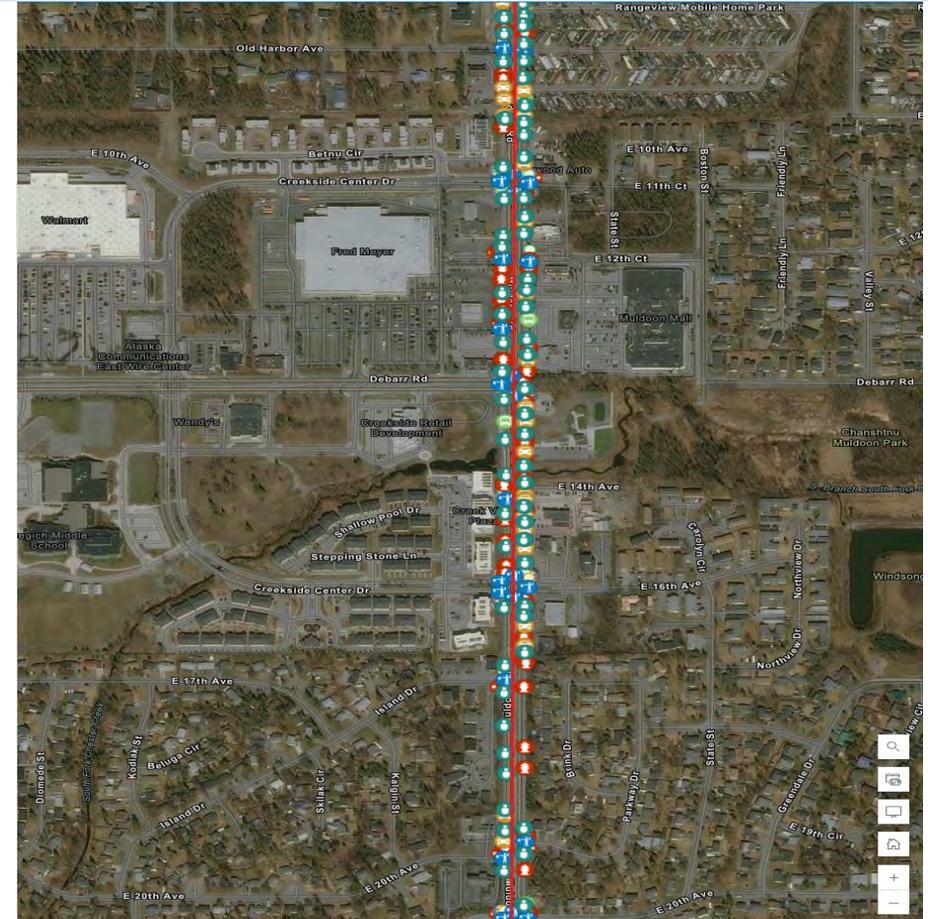
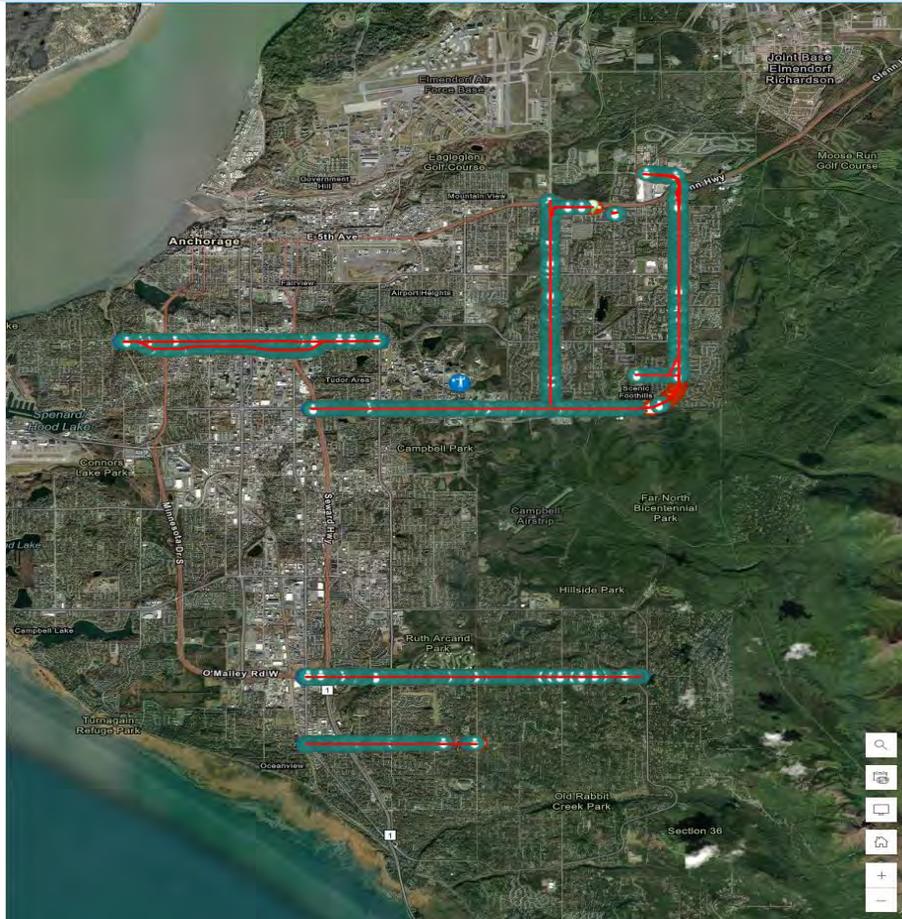
Table Edit Get directions Zoom to

Description of vertical change in level	
Curb Counter Slope	4.50
Depressed Curb Slope	6.20
Road Running Slope	2.50
Ramp Running Slope	7.90
Ramp Cross Slope	1.40
Upper Landing Running Slope	1.00
Upper Landing Cross Slope	0.10
Left Flare Slope	1.80
Right Flare Slope	5.80
Are there any protrusions?	No
Type of protrusion	
Distance protrusion extends into walkway	
Width of clear space around protrusion	
Lowest edge of protrusion	
Is there a Detectable Warning?	Yes
Is the Detectable Warning a contrasting color from the rest of the curb/ramp?	Yes
Does the Detectable Warning have borders greater than 2"?	No
Detectable Warning Rating	Between 0% & 10% damaged
Detectable Warning Description	Fair
Is there grating?	No
Is the longest side of the grating opening perpendicular to the direction of travel?	
Is the grating opening greater than 1/2"?	





# Anchorage 2025 Map





Any  
Questions

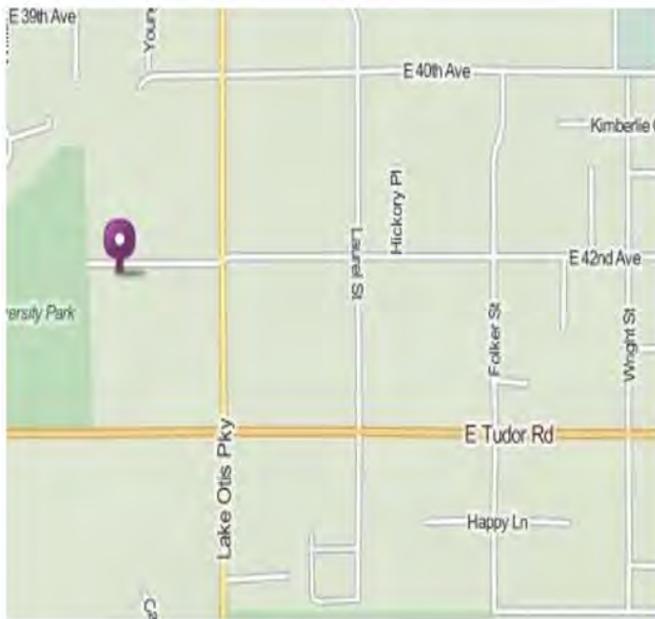
# Contact Information

**Michael Dupuis**

Alaska DOT&PF Civil Rights Office

Research Analyst

Michael.Dupuis@alaska.gov



Alaska DOT&PF  
Civil Rights Office  
2200 E. 42<sup>nd</sup> Ave  
PO Box 196900  
Anchorage, AK 99519-6900

Phone: (907) 269-0851

Fax: (907) 269-0847

[www.dotcivilrights.alaska.gov](http://www.dotcivilrights.alaska.gov)

